



**Leicester**

# Walking and Cycling Index 2025



# Our vision for walking, wheeling and cycling



**Sir Peter Soulsby**  
City Mayor  
Leicester City Council

I am delighted to introduce the 2025 Walking and Cycling Index report for Leicester.

This is the first monitoring and evaluation report in partnership with Walk Wheel Cycle Trust (formerly Sustrans) to help track progress in delivering walking, wheeling and cycling improvements for our city.

Walk Wheel Cycle Trust has actively contributed to local improvements for over 30 years. They helped establish two national cycle routes linking the city north, south, east and west with neighbours in the surrounding districts. These routes now meet at Leicester Clock Tower in the UK's largest Pedestrian Priority Zone providing a safe and welcoming city centre for people of all ages, abilities and communities.

Future reports will be published every two years and we'll be able to compare with similar cities across the UK and the Republic of Ireland

using the Index's data tool. It will form part of our Heart of Leicester Plan guiding development ambitions over the next 10 years.

Leicester is ideally sized for getting around by walking, using mobility aids or by bicycle. Most journeys are under 2km and public transport options are growing as the city tackles barriers and gaps in provision that prevent some people and communities from sharing our wonderful streets and public places.

Over the coming years we will focus on integration with public transport options, especially buses and the railway. We will address road dangers and safety fears, especially around schools and in neighbourhoods. We will continue to implement 20mph Zones across the city and create a network of citywide safe, attractive and direct walking, wheeling and cycling routes.

The Leicester Walking and Cycling Index will help to monitor, share and target progress across the whole city.

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## The Walking and Cycling Index

The Walking and Cycling Index is the biggest assessment of walking, wheeling and cycling in the UK and Ireland. It is delivered by Walk Wheel Cycle Trust (formerly Sustrans) in collaboration with 22 regional and local partners. Each area reports on the progress made towards making walking, wheeling and cycling a more attractive, everyday way to travel.

The Walking and Cycling Index reports every two years. This is the first report from Leicester, produced in partnership with Leicester City Council. The data in this report comes from 2025 and includes local walking, wheeling and cycling data, modelling and an independent, demographically representative survey of 1,250 residents aged 16 or above. The survey was conducted from March to June 2025. Social research organisation NatCen conducted the survey, which is representative of all residents, not just those who walk, wheel or cycle.

Our thanks to the people of Leicester who took part in the survey and shared their stories with us.

More details on all reports and an interactive data tool to explore the data in more depth can be found at [www.walkwheelcycletrust.org.uk/walking-cycling-index](http://www.walkwheelcycletrust.org.uk/walking-cycling-index)



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## Defining wheeling

Some people who use a wheelchair or a mobility scooter may not identify with the term walking and may prefer to use the term wheeling. We use the terms walking and wheeling together to ensure we are as inclusive as possible.

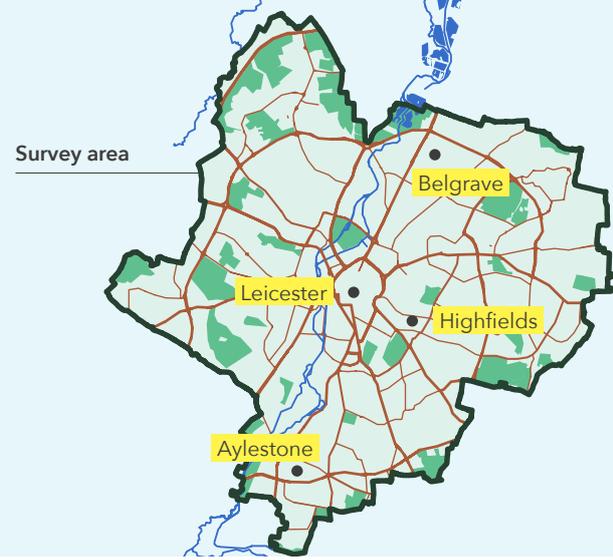
## Headlines

# Leicester

Population<sup>[i]</sup>

**388,348**

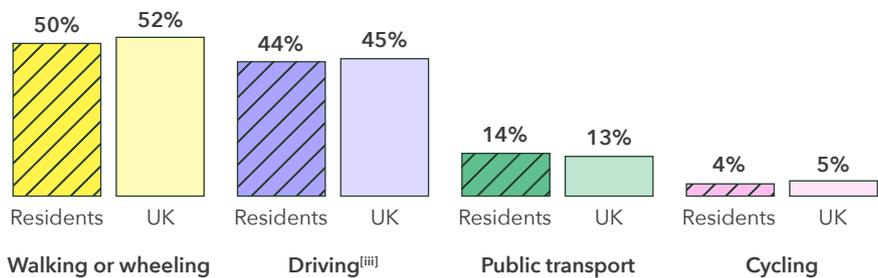
**308,008 adults**  
**80,340 children**



## Frequency of different modes of travel

Walking and wheeling is the most common mode of travel amongst Leicester residents.

Residents who travel by the following modes five or more days a week in the city compared with other parts of the UK<sup>[ii]</sup>



i. ONS mid-year 2024 population estimates. This is the most recent available for Leicester.

ii. UK aggregate is calculated using all participating UK Walking and Cycling Index areas.  
iii. Travelling as driver or passenger of car, van or motorcycle.

## Walking, wheeling and cycling participation is not equal

Residents' travel choices and their perceptions of walking, wheeling and cycling sometimes vary widely between different groups.

**50%**

of residents walk or wheel at least five days a week

Proportion of residents who walk or wheel at least five days a week

44% of disabled people

51% of non-disabled people

**13%**

of residents cycle at least once a week

Proportion of residents who cycle at least once a week<sup>[iv]</sup>

8% of women

18% of men

## Not all residents feel safe and welcome in their neighbourhood

Proportion of residents who think it's safe to walk or wheel in their local area

65% of people aged 16-25

69% of people aged 66+

Proportion of residents who think it's safe to cycle in their local area

46% of women

50% of men

Proportion of residents who feel welcome and comfortable walking, wheeling or spending time on the streets of their neighbourhood

60% of disabled people

69% of non-disabled people

iv. See footnote on page 6.

## Everyone benefits when more people walk, wheel and cycle

Based on modelling of survey data, every year, walking, wheeling and cycling:

prevents

**610**

serious long-term health conditions

creates

**£235.3 million**

in economic benefit for individuals and the city

saves

**16,000 tonnes**

of greenhouse gas emissions

Walking, wheeling and cycling reduces congestion by taking up to 95,000 cars off the roads every day.

## Residents want to walk, wheel and cycle more and drive less

Percentage of residents who would like to use different types of transport more or less in the future:

18% of residents want to drive less. 41% of residents agree that they often use a car because no other transport options are available.

Walk or wheel

56% more

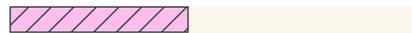


4% less



Cycle

43% more



5% less



Take public transport

32% more



12% less



Drive

25% more



18% less



## Cycling is seen as less safe than other forms of transport

Percentage of residents who think it's safe in their local area to:

67% walk or wheel



47% cycle



72% use public transport



77% drive



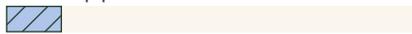
## Residents support more connected neighbourhoods

55% of residents support shifting investment from road-building schemes to fund walking, wheeling, cycling and public transport. 16% oppose such a shift.

69% support



13% oppose

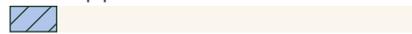


stopping vehicles parking on the pavement

73% support



11% oppose

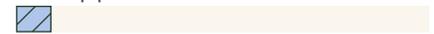


improving and increasing off-road walking, wheeling and cycling paths

70% support



8% oppose

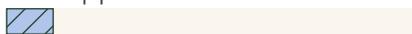


improving walking, wheeling and cycling access to bus stops and coach or rail stations

70% support



11% oppose

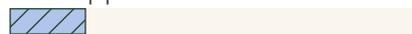


reducing speed limits, improving crossing points and introducing protected cycle paths in school neighbourhoods

58% support



18% oppose

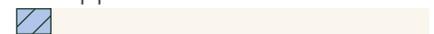


closing residential streets outside schools to cars during drop-off and pick-up times

75% support



8% oppose



having shops, schools, green space and public transport within a short walk or wheel of their home<sup>v</sup>

v. Local places with services and amenities nearby your home are often known as 20-minute neighbourhoods.



## Participation, safety and satisfaction

### Walking and wheeling participation

Walking or wheeling is the most common mode of travel for short journeys and is vital for travelling to and from public transport.

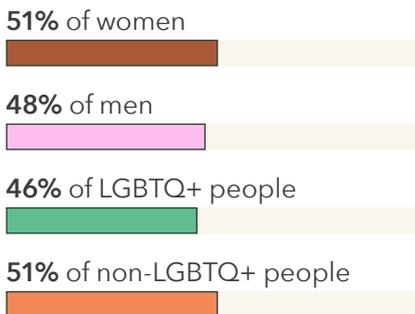
Encouragingly, the majority of residents feel it's safe to walk or wheel in their local area and think it is a good place to walk or wheel.

**93%**  
of all residents  
walk or wheel

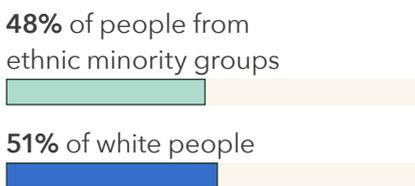
**50%**  
of all residents  
walk or wheel  
at least five  
days a week

### Proportion of residents who walk or wheel at least five days a week

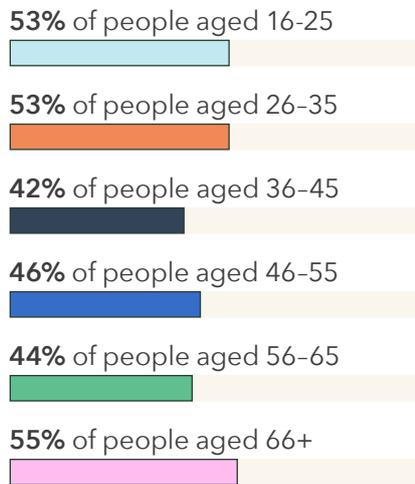
#### Gender and sexuality<sup>[i]</sup>



#### Ethnicity



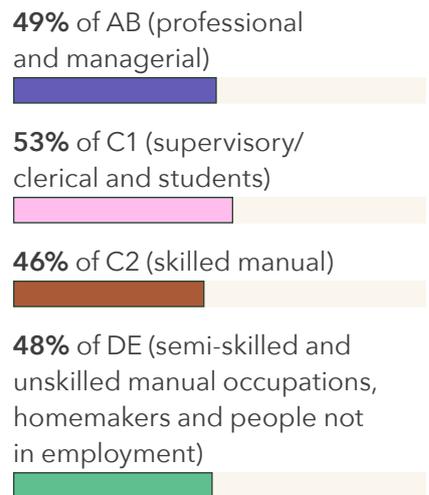
#### Age



#### Disability



#### Socio-economic group<sup>[ii]</sup>



i. The sample size for respondents who identified their gender 'in another way' was too low to be statistically significant and is not shown.  
 ii. Socio-economic group is a classification based on occupation maintained by the Market Research Society.

## Walking and wheeling safety and satisfaction

**67%**

of all residents think it's safe to walk or wheel in their local area

**52%**

of all residents think it's safe for children to walk or wheel in their local area

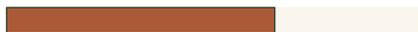
**73%**

of all residents think their local area overall is a good place to walk or wheel

### Proportion of residents who think it's safe to walk or wheel in their local area

#### Gender and sexuality

65% of women



69% of men



62% of LGBTQ+ people



68% of non-LGBTQ+ people



#### Ethnicity

69% of people from ethnic minority groups



64% of white people



#### Age

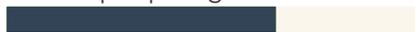
65% of people aged 16-25



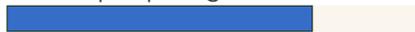
70% of people aged 26-35



65% of people aged 36-45



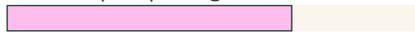
74% of people aged 46-55



60% of people aged 56-65



69% of people aged 66+



#### Disability

61% of disabled people

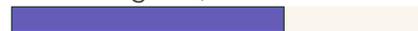


69% of non-disabled people



#### Socio-economic group

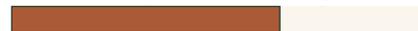
66% of AB (professional and managerial)



70% of C1 (supervisory/ clerical and students)



65% of C2 (skilled manual)



66% of DE (semi-skilled and unskilled manual occupations, homemakers and people not in employment)



### Aminah Shaikh - Mature student

I came to Leicester from Pakistan in 2023 to do a PhD in Economics.

Back home, I always drove, but here I couldn't manage the cost and hassle of a car.

I had stomach and blood pressure issues when I arrived, but it was hard to get a doctor's appointment. I started walking the 45 minutes to university every day and by the time I saw a doctor, my health issues had gone.

I'd thought medication was just part of ageing, but this experience changed my mindset. Walking improved my health, and I realised age really is just a number.

# Participation, safety and satisfaction

## Cycling participation

There is great potential for cycling in Leicester with around half of residents thinking it's safe to cycle in their local area and that the local area overall is a good place to cycle.

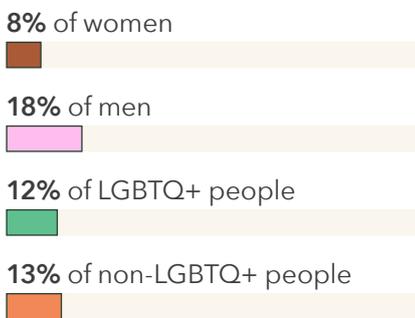
Safety, including road safety and personal safety, is the single largest barrier to cycling.<sup>[i]</sup>

**34%**  
of all residents cycle

**13%**  
of all residents cycle at least once a week

## Proportion of residents who cycle at least once a week

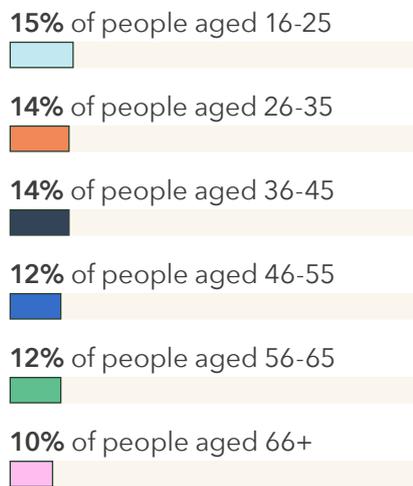
### Gender and sexuality



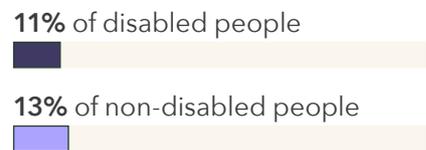
### Ethnicity



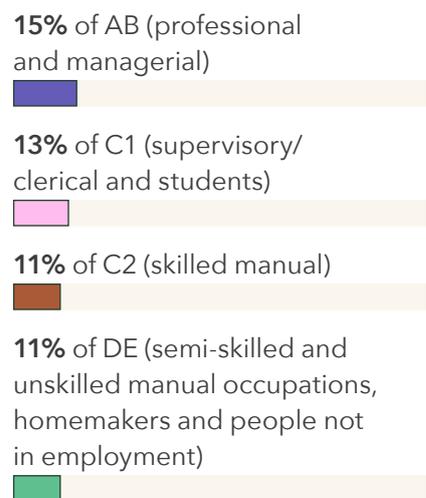
### Age



### Disability



### Socio-economic group



i. See Bike Life 2019 UK report.

## Cycling safety and satisfaction

**47%**

of all residents think it's safe to cycle in their local area

**39%**

of all residents think it's safe for children to cycle in their local area

**51%**

of all residents think their local area overall is a good place to cycle

### Proportion of residents who think it's safe to cycle in their local area

#### Gender and sexuality

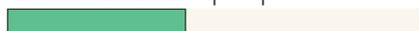
46% of women



50% of men



43% of LGBTQ+ people



49% of non-LGBTQ+ people



#### Ethnicity

51% of people from ethnic minority groups



43% of white people



#### Age

48% of people aged 16-25



52% of people aged 26-35



44% of people aged 36-45



56% of people aged 46-55



47% of people aged 56-65

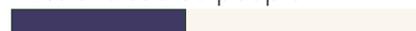


36% of people aged 66+



#### Disability

42% of disabled people



49% of non-disabled people



#### Socio-economic group

43% of AB (professional and managerial)



50% of C1 (supervisory/ clerical and students)



44% of C2 (skilled manual)



50% of DE (semi-skilled and unskilled manual occupations, homemakers and people not in employment)



### Chris Slowe - Programme Coordinator

For the past four years, I've cycled to work daily using the same route, leaving my bike in the secure cage for building tenants. I think the city council did a great job with the Granby Street bike lane; I like how it cleverly navigates tricky road sections. It's useful and makes getting into town by bike simple.

I'm a director at All-In Access-Ability - an accessibility organisation. We assess accessibility across Leicester, improving venues and their access from the street - sometimes encouraging improvements from the council such as adding a dropped kerb or influencing infrastructure decisions, improving access for everyone.

## Benefits of walking and wheeling

# Why everyone gains when more people walk or wheel



### Together, Leicester residents walk or wheel 14 times around the world every day

Walking and wheeling improves health and air quality, saves the NHS money, and reduces congestion.

Residents made

**150.7 million**

walking and wheeling trips  
in 2025

This adds up to

**128.2 million  
miles per year**

or

**350,000 miles  
per day**

This equates to each  
resident spending

**4 days**

walking or wheeling  
continuously in the past year

### Annual walking and wheeling trips by purpose

Enjoyment or fitness,  
including running  
(adults and children):

60,400,000

40%

Destination – such as work,  
school, shopping (adults):

81,400,000

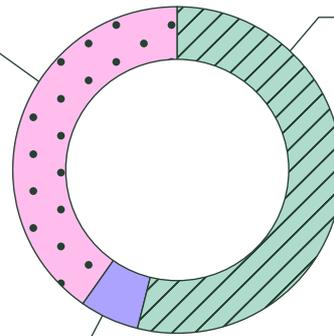
54%

School (children):

8,900,000

6%

Average walking or wheeling  
trip distance: 0.9 miles



### Walking and wheeling benefits residents and society

We model the costs and benefits of walking and wheeling journeys, including those that replace trips which could have been driven, using our Societal Gain model. This includes travel time, vehicle operating costs, health benefits, air quality and taxation.

The total annual economic  
benefit from all trips walked  
and wheeled in Leicester is

**£188.4 million**

Many of these trips were made by people with access to a car. For these (excluding trips for enjoyment or fitness) our model compares the costs and benefits of walking and wheeling to those of driving.

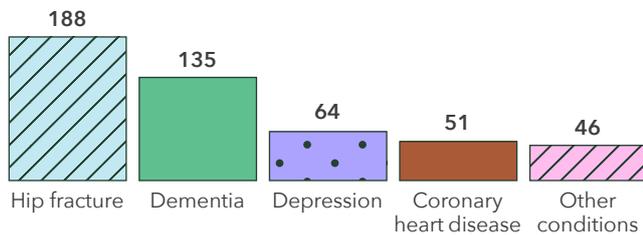
For these journeys **92p** is saved for each mile walked or wheeled instead of driven. Over a year this adds up to **£35.2 million** in economic benefit for residents and society from **38.1 million miles** walked or wheeled by those that could have used a car.



## Walking and wheeling unlocks health benefits for everyone

Walking in the city prevents 484 serious long-term health conditions each year.

### Cases prevented



This saves the NHS

**£6.2 million per year**

Equivalent to the cost of

**140,000  
GP appointments**

Above figures are based on applying city data to Sport England MOVES tool, which shows the return on investment for health of sport and physical activity. 'Other conditions' includes type 2 diabetes, stroke, breast cancer and colorectal cancer.

The physical activity benefits from walking

**prevent 126 early  
deaths annually**

which is valued at

**£518 million**

Based on WHO/Europe Health Economic Assessment Tool (HEAT), which enables an economic assessment of the health benefits of walking by estimating the value of reduced mortality resulting from specified amounts of walking. Wheelchair and mobility scooter trips are modelled as walking trips for the purposes of the MOVES and HEAT models.

People walking and wheeling instead of driving improves air quality, saving:

**25,000 kg of NO<sub>x</sub>**  
(Nitric oxide and nitrogen dioxide)

and

**2,200 kg of particulates**  
(PM<sub>10</sub>, which includes PM<sub>2.5</sub>) annually.

41% of residents agree the air  
is clean in their local area



## Walking and wheeling helps mitigate the climate crisis

By walking or wheeling instead of driving, residents save

**10,000 tonnes**

of greenhouse gas emissions  
(carbon dioxide, methane and nitrous oxide) annually.

This is equivalent to the  
carbon footprint of

**19,000 people  
taking flights**

from East Midlands to Tenerife.

In 2023 domestic transport accounted for **29%** of the UK's greenhouse gas emissions, of which the main sources are the use of petrol and diesel in road transport.

Department for Energy Security and Net Zero, 2023 UK Greenhouse Gas Emissions, Final Figures.

## Walking and wheeling keeps the city moving

Studies show walking or cycling frees up road space in comparison to driving.<sup>[1]</sup> This helps to keep the city moving for all road users.

**84,000 return trips**

are walked and wheeled daily by residents who could have used a car.

If these cars were all in a traffic jam it would tail back

**251 miles**

equivalent to the distance from Leicester to Liverpool and back.

i. Litman, 2023. Evaluating Transportation Land Use Impacts. Based upon Eric Bruun and Vuchic, 1995. The Time-Area Concept.

## Benefits of cycling

# Why everyone gains when more people cycle



## Together, Leicester residents cycle 5 times around the world every day

Cycling can be convenient for many local trips, especially in cities and towns. It gives people transport choice and access to education, jobs and community.

Residents made

**13.9 million**  
cycling trips in 2025

This adds up to

**41.8 million miles**  
per year  
or  
**110,000 miles**  
per day

### Annual cycling trips by purpose

Leisure (adults  
and children):  
**2,400,000**  
17%

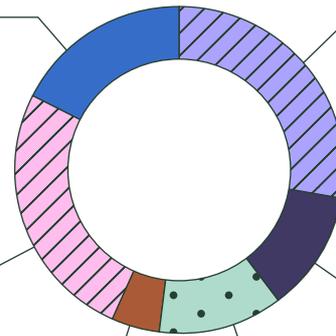
Shopping,  
personal  
business  
and social  
trips (adults):  
**3,600,000**  
26%

School (children):  
**660,000**  
5%

To work (adults):  
**3,900,000**  
28%

Part of  
work (adults):  
**1,600,000**  
12%

School, college or  
university (adults):  
**1,700,000**  
12%



Average cycling trip distance: 3 miles

## Cycling benefits residents and society

We model the costs and benefits of cycling journeys, including those that replace trips which could have been driven, using our Societal Gain model. This includes travel time, vehicle operating costs, health benefits, air quality and taxation.

The total annual economic benefit from all trips cycled in Leicester is

**£46.8 million**

Many of these trips were made by people with access to a car. For these (excluding trips for leisure) our model compares the costs and benefits of cycling to those of driving.

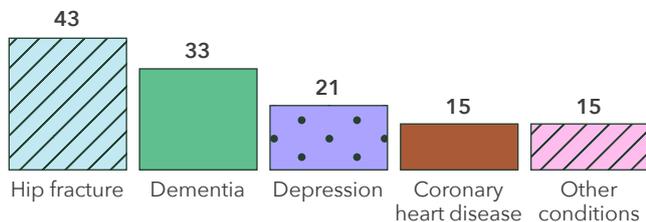
For these journeys **£1.63** is saved for each mile cycled instead of driven. Over a year this adds up to **£33.9 million** in economic benefit for residents and society from **20.8 million miles** cycled by those that could have used a car.



## Cycling unlocks health benefits for everyone

Cycling in the city prevents 127 serious long-term health conditions each year.

### Cases prevented



This saves the NHS

**£1.7 million per year**

Equivalent to the cost of

**37,000  
GP appointments**

Above figures are based on applying city data to Sport England MOVES tool, which shows the return on investment for health of sport and physical activity. 'Other conditions' includes type 2 diabetes, stroke, breast cancer and colorectal cancer.

The physical activity benefits from cycling

**prevent 13 early  
deaths annually**

which is valued at

**£54.6 million**

Based on WHO/Europe Health Economic Assessment Tool (HEAT), which enables an economic assessment of the health benefits of cycling by estimating the value of reduced mortality resulting from specified amounts of cycling.

People cycling instead of driving improves air quality, saving:

**8,900 kg of NO<sub>x</sub>**  
(Nitric oxide and nitrogen dioxide)

and

**963 kg of particulates**  
(PM<sub>10</sub>, which includes PM<sub>2.5</sub>) annually.

41% of residents agree the air  
is clean in their local area



## Cycling helps mitigate the climate crisis

By cycling instead of driving, residents save

**5,600 tonnes**

of greenhouse gas emissions  
(carbon dioxide, methane and nitrous oxide) annually.

This is equivalent to the carbon footprint of

**11,000 people  
taking flights**

from East Midlands to Tenerife.

In 2023 domestic transport accounted for **29%** of the UK's greenhouse gas emissions, of which the main sources are the use of petrol and diesel in road transport.

Department for Energy Security and Net Zero, 2023 UK Greenhouse Gas Emissions, Final Figures.

## Cycling keeps the city moving

Studies show walking or cycling frees up road space in comparison to driving.<sup>[1]</sup> This helps to keep the city moving for all road users.

**11,000 return trips**

are cycled daily by residents who could have used a car.

If these cars were all in a traffic jam it would tail back

**32 miles**

equivalent to the distance from Leicester to Royal Leamington Spa.

i. Litman, 2023. Evaluating Transportation Land Use Impacts. Based upon Eric Bruun and Vuchic, 1995. The Time-Area Concept.

## Walking and wheeling solutions

# What would help make walking and wheeling easier?

### New homes and existing communities should have services and amenities within walking and wheeling distance

Putting walking and wheeling at the heart of new and existing communities gives people greater transport choice, while reducing car dependency.

**58%**

agree they can easily get to many places they need to visit without having to drive

**56%**

support stopping new housing developments in areas where driving is the only practical option to visit local services, such as shops and doctors surgeries

New developments and existing communities can support walking and wheeling by ensuring many of the things people need are found near to where people live.

What percentage of residents would find more local amenities and services useful to help them walk or wheel more?

**83%**

more shops and everyday services, such as banks and post offices, close to their home

**79%**

more government services, such as doctors surgeries and schools, close to their home

**86%**

more parks or green spaces close to their home

**82%**

more things to see and do close to their home, like cafés or entertainment venues

### Residents want walking and wheeling to better align with buses

While walking and wheeling should be the most attractive option for short journeys, it should also be integrated with public transport for longer trips.

Walking and bus use is the most common form of multi-modal journey across the UK. If we are to increase walking and wheeling and bus trips, residents told us they need practical improvements to better integrate these modes of transport.



What percentage of residents would find these changes useful to help them take journeys that include walking or wheeling and the bus more often?

**80%**

more information to help them plan and take a journey (such as apps, maps, signage or public transport staff)

**76%**

more direct walking and wheeling routes from housing estates to bus stops

**76%**

accessible walking and wheeling routes to and from bus stops

**82%**

improved safety while walking or wheeling to and from the bus stop

**82%**

improved walking and wheeling crossing facilities near bus stops

**85%**

improved bus stops (such as accessible, well lit and maintained, cover from rain, display of service information)

## Residents want better streets

There are many ways to make our streets and neighbourhoods safe, welcoming and comfortable for everyone to walk or wheel in.

### What percentage of residents think that these changes would help them walk or wheel more?

**76%**  
wider pavements

**79%**  
more frequent road crossings, with reduced wait times

**84%**  
nicer places along streets to stop and rest, like more benches, trees and shelters

**78%**  
better pavement accessibility, like level surfaces and dropped kerbs at crossing points

**70%**  
fewer cars parked on the pavement

**77%**  
less fear of crime or antisocial behaviour in their area

### In the city, for each road making up a junction with traffic lights

85% have a pedestrian crossing



15% don't have a pedestrian crossing



For example, a T-junction has three roads making up the junction, and a crossroads has four.



### Sue Willis - Engagement Coordinator

The Canal & River Trust are currently involved in "Restoring the Soar", a river restoration project in partnership with Leicester City Council and Leicestershire & Rutland Wildlife Trust. Findings from the consultation with stakeholders showed that although a lot of people live near the waterway, many don't walk along the river, so we're working to understand some of the reasons behind that.

Safety and litter are two key reasons. To improve this, a range of measures will be introduced including tackling

anti-social behaviour, making changes to the physical environment e.g. by improving sight lines, and exploring measures to tackle litter at its source.

We hope these improvements, as well as more organised group walks along the waterway, will increase footfall in the area which in turn will make people feel safer. It is a wonderful traffic-free blue-green space with huge potential benefits to mental and physical health that should be enjoyed by everyone.

## Cycling solutions

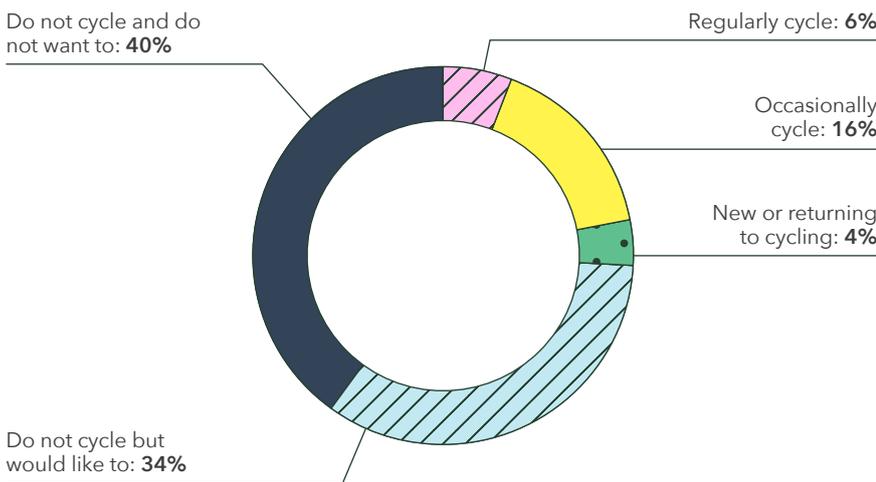
# What would make cycling better?

### Many residents want to cycle

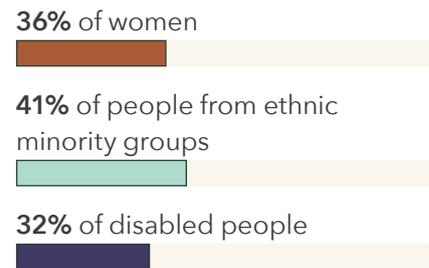
There is considerable appetite to start cycling from residents who don't already.

To meet this demand we must address safety, ensuring people have dedicated space for cycling, as well as supporting access to a cycle, training, cycle parking, and better integration of cycling with public transport.

#### How do residents see themselves when it comes to cycling?

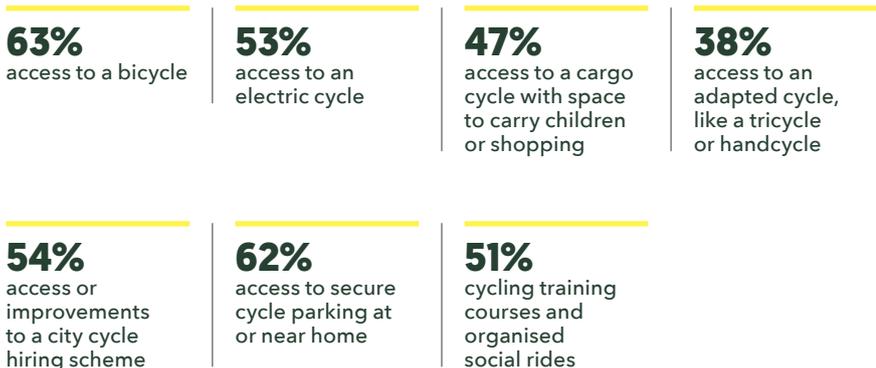


#### What proportion of residents said they 'do not cycle but would like to'



### Residents want more support to cycle

#### What percentage of residents think that these kinds of support would help them start cycling or to cycle more?



#### Proportions of residents with access to an adult cycle



## Residents want improved cycling infrastructure

People want networks of dedicated and safe cycle paths, cycle parking and better integration with public transport.

### In the city there are:

**45 miles**

of traffic-free cycle paths away from the road

**19 miles**

of cycle paths physically separated from traffic and pedestrians

**7 miles**

of signposted routes along quieter streets

**136**

cycle parking spaces across 4 railway and bus stations<sup>i)</sup>

### What percentage of residents would be helped to start cycling or to cycle more by better facilities?

**69%**

more traffic-free cycle paths away from roads, like through parks or along waterways

**66%**

more cycle paths along roads that are physically separated from traffic and pedestrians

**67%**

more signposted local cycle routes along quieter streets

**71%**

better links with public transport, like secure cycle parking at train stations, bus stops/stations

### Among residents:

**73%**

support improving and increasing local off-road walking, wheeling and cycling paths

**56%**

support building more cycle paths physically separated from traffic and pedestrians, even when this would mean less room for other road traffic

**70%**

support improving walking, wheeling and cycling access to bus stops and coach or rail stations

i. Includes cycle parking at Park and Ride stations.

## Isaura Medina - Warehouse Operative

At sixty, I decided to try something new and emailed a contact from an old business card I found in a bike shop offering free cycling lessons. Amazingly I got a response even after all that time!

The sessions weren't running at the time, but I kept an eye on social media and signed up when sessions restarted. I was shy and a little bit scared at first, but seeing other women there helped.

I went every Saturday for five weeks. As soon as it clicked, I felt invincible. Even falling didn't faze me. It taught me that everything is possible if we put our mind to it.



## Thriving places, centring children



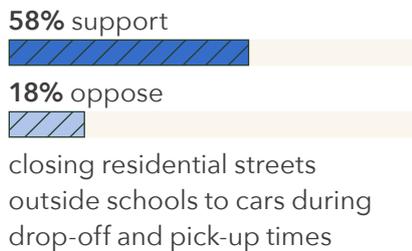
Neighbourhoods should prioritise the needs and wellbeing of the people who live there and visit. They should be planned to support community, foster social connection, and be places where everyone feels welcome.

Designing neighbourhoods with children in mind means they work better for everyone.

### Improving the journey to school

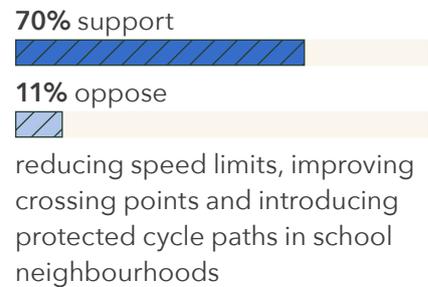
Schools are at the centre of many neighbourhoods and should be part of any plan to make neighbourhoods better.

This means working with schools to encourage walking, wheeling and cycling, but also improving the neighbourhood around schools to reduce traffic and improve pavements, crossing points and cycle provision.



These are known as 'School Streets'.

**6 schools** in the city have School Streets schemes



### Giving children the independence to roam, play and develop

Neighbourhoods should be places for children to thrive. Well-designed neighbourhoods can give children freedom to move around, play and socialise, helping children's independence, social skills, and autonomy.

Average age when parents and carers would let children travel independently in their neighbourhood

**12 years old** to walk or wheel

**13 years old** to cycle

**14 years old** to use public transport

**48%** agree there is space for children to socialise and play

Fewer children play out on their streets than ever before. Just **27%** of children said they regularly play outside their homes, compared to **71%** of those born between 1946 and 1964.

Save the Children, Summer of Play campaign survey, 2022.



## Residents support child-safe neighbourhoods

Adults support child-safe neighbourhoods, including traffic-reduction targets and measures to reduce through traffic and traffic speed, but this must be done fairly.

**32%**

of residents think that their streets are not dominated by moving or parked motor vehicles

**61%**

support setting traffic-reduction targets and taking action to achieve these

**63%**

support low-traffic neighbourhoods

**33%**

of the total length of unclassified roads in the city have nothing to prevent through traffic. This can result in rat running.<sup>[i]</sup>

Residents would find fewer motor vehicles on their streets useful to:

68% walk or wheel more



60% cycle more



**59%**

support reducing speed limits on local roads in built-up areas to 20 mph speed limits

**46%**

of the city's streets have 20 mph speed limits (excluding motorways)

Residents would find more streets with 20 mph speed limits useful to:

59% walk or wheel more



57% cycle more



## Everyone should feel welcome in their neighbourhood

Improvements to neighbourhoods must also recognise the needs of adults, for example women, disabled people and people from ethnic minority groups.

**50%**

agree they regularly chat to their neighbours, more than just to say hello

**49%**

feel able to participate in making their neighbourhood a better place to live

Proportion of residents that feel welcome and comfortable walking, wheeling or spending time on the streets of their neighbourhood

**67%**

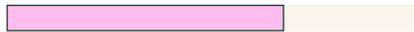
of all residents



67% of women



67% of men



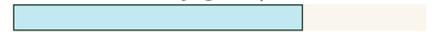
60% of LGBTQ+ people



68% of non-LGBTQ+ people



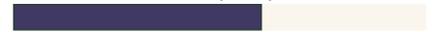
70% of people from ethnic minority groups



63% of white people



60% of disabled people



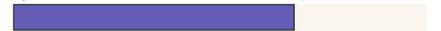
69% of non-disabled people



68% of socio-economic group DE (semi-skilled and unskilled manual occupations, homemakers and people not in employment)



68% of socio-economic group AB (professional and managerial)



i. Based on analysis by CycleStreets [www.lowtrafficneighbourhoods.org](http://www.lowtrafficneighbourhoods.org). Unclassified roads are all public roads that are neither motorways, A, B nor C roads.

**Developing the city****Improving walking,  
wheeling and cycling**

We continue to invest in infrastructure and initiatives across the city that support walking, wheeling and cycling.

The completion of the A50 Fiveways projects means it is now possible to cycle between the city centre and Glenfield entirely separated from traffic, offering a safer and more enjoyable journey. Alongside this, new routes through the Rally and Duns Lane improve access to De Montfort University and, via the Great Central Way, Aylestone Meadows and beyond.

Reducing vehicle speed remains a priority. Our neighbourhood 20 mph programme has now reached 60% of the city's streets, with a target of 80% by 2027. These schemes are making a real difference to neighbourhoods across the city, by slowing traffic, discouraging rat-running, and creating more welcoming places for people to spend time.

Our investment in the city's passenger transport network continues to allow us to better serve and connect our communities.

Most buses operating within the city are now electric, and investment from Bus Service Improvement Plan Grant will expand electric services on routes across the city and into our neighbouring suburbs – improving efficiency, reliability and air quality whilst reducing transport emissions.



**These schemes are making a real difference to neighbourhoods across the city... creating more welcoming places for people to spend time.**



Finally, our new e-bike loan scheme offers residents a free month-long trial, with discounts available for those on low incomes. Combined with training, group rides, and flexible loan options, these programmes make it easier than ever for people to travel actively for more of their journeys.



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## Leicester eCycles Project (2025)

Launched in May 2025 with funding from Active Travel England, the project offers 160 free one-month eCycle loans to residents, plus a £300 purchase voucher. Demand has been high, improving activity levels and reducing car trips.

One participant said:

“I’d been looking for something fun to help me and the kids get more active together. I think the scheme’s a brilliant idea. You get plenty of time to really try out the bike, see what it can do, and figure out how it could fit into your everyday life. After taking part, I’d definitely think about buying an e-bike of my own. I’ve already been checking out prices and models, and I’m hoping to get one next year if the budget allows.”



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## Social Inclusion Project

The Social Inclusion Project helps under-represented communities access entry-level cycling. It includes cycle delivery and library services, supports Rides in the Park with Wheels for All, and contributes to cultural and charity events.

In 2025, project activities resulted in more than 1,100 people joining 40 led rides, with 6,500 people taking part in festivals, events and the Ride Leicester programme.

One participant said:

“I have enjoyed the group rides being with other ladies who are just like me trying to build up their confidence. I used to ride a long time ago when my children were young. If you don’t do it you just keep thinking I can’t do it. So, if people have ridden, please come back and ride again. And those that haven’t, try to.”

# Better places and streets for everyone

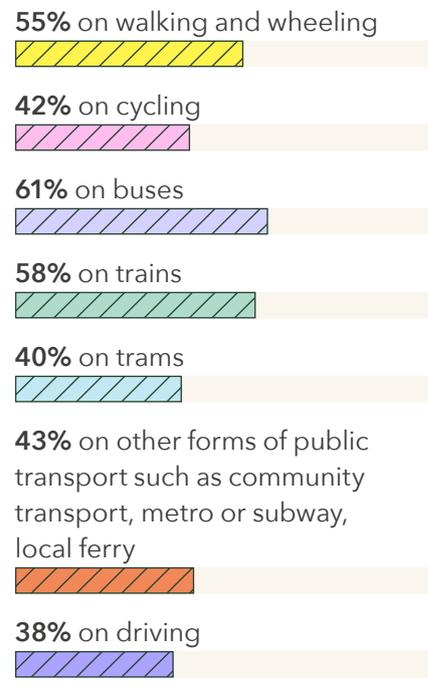


With the publication of the Local Cycling and Walking Infrastructure Plan (LCWIP), we are setting the direction for the next generation of active travel schemes across the city.

There will be new corridor schemes along major routes to better connect areas of the city. Local neighbourhood improvements will create more space for people to enjoy their communities and improve their access to and from local services, schools, and bus stops – enabling more accessible multi-modal journeys. The LCWIP features a core set of priorities for delivery over the next ten years and will be continually reviewed to ensure it focuses on improvements that provide the most benefit to journeys across the city.

A central focus of our future delivery is to support children and young people, and their families to travel more sustainably. The continued expansion of the School Streets initiative and our pedestrian crossing programmes, prioritised where the opportunity for the greatest local impact, will improve safety around the school gates, reduce congestion, and give children and young people a healthy start to the school day. This initiative is vital to our emerging Sustainable Modes of Travel to School strategy – alongside supporting businesses and facilities that are at the heart of our neighbourhoods, and will help us to embed long-term travel behaviour change.

Percentage of residents who would like to see more government spending in their local area:



**A central focus of our future delivery is to support children and young people**

Future schemes also recognise the importance of accessibility to recreation and cultural venues. The delivery of new projects in the cultural quarter, along with improvements across both The Rally and Stokeswood parks, alongside other projects in development across the city, will expand the city's network of safe active travel routes and serve to better connect our entertainment and cultural venues. These developments will build on our established network to create stronger connections between the places where people live, learn, relax, and socialise.

We are also eagerly awaiting the opportunity to align our ambitions with the forthcoming publication of the government's new Integrated National Transport Strategy and the third Cycling and Walking Investment Plan, to demonstrate how we can contribute to the national picture for transport moving forward. These national policies will provide clarity on future funding, and help ensure that local programmes contribute positively to the wide UK plans for cleaner, more efficient, and reliable transport networks. By combining strong local partnerships, clear planning and community engagement, we are excited to deliver a network that benefits everyone who lives, works, or spends time in the city of Leicester.



### Chris Dee — Director at Northside Bikes

I've been in Leicester for 14 years and cycled most of that time. The city has changed a lot – there wasn't much cycling infrastructure but now there are miles of paths, and far more people using them.

In my area, many cyclists are from low-income households and diverse backgrounds, as they're realising that cycling is safer, cheaper, and accessible.

Looking forward, the people who will continue to use bikes will be across all demographics.

However women's participation still lags despite council funding and projects to improve balance.

Infrastructure out of the city centre is well established; we now need routes which connect communities without going through the city centre because currently, getting to communities in the north and east of the city is a challenge.

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## Notes on methodology

The attitudinal survey was conducted from March to June 2025 by independent social research organisation NatCen.

The survey is representative of all Leicester residents, not just those who walk, wheel or cycle.

All other data are sourced from our regional and local partners, national data sets or modelled and calculated by Walk Wheel Cycle Trust. All locally-sourced data are correct as of 28 May 2025.

Trip estimates use a model developed by Walk Wheel Cycle Trust. When comparing to other travel surveys, some variation may exist in the proportion of journeys travelled by journey purpose.

Rounding has been used throughout the report. In many cases we have rounded to the nearest whole number. Rounding is avoided where this may cause confusion, for example with modelled estimates shown in the summary and benefits sections.

More information on data sources and calculations, including strengths and limitations of the survey and model methodology, are available at [www.walkwheelcycletrust.org.uk/walking-cycling-index](http://www.walkwheelcycletrust.org.uk/walking-cycling-index)

Walk Wheel Cycle Trust is the charity making it possible for everyone to walk, wheel and cycle.

Because people powered movement changes everything. Our health. Our wellbeing. Our world.

[www.walkwheelcycletrust.org.uk](http://www.walkwheelcycletrust.org.uk)

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## Funding and support

Walking and Cycling Index Leicester has been funded and supported by The Freshfield Foundation and Leicester City Council. The project is co-ordinated by Walk Wheel Cycle Trust.

Walk Wheel Cycle Trust is a registered charity in the UK number 326550 (England and Cymru), SC039263 (Scotland) and 20206824 (Republic of Ireland).

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